

URBAN DESIGN BRIEF

117 Bloomingdale Road

City Of Kitchener
Zoning Bylaw Amendment

August 2023



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Prepared for:
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1. BACKGROUND

1.1 Proposal

GSP Group Inc. has been retained by Marann Homes (the “Applicant”) for the property at 117 Bloomingdale Road (the “Subject Property”) in the City of Kitchener. The proposed development includes four 3-storey stacked townhouse buildings containing a total of 40 units with outdoor amenity spaces and 46 surface parking spaces.

1.2 Purpose and Outline

The purpose of this Urban Design Brief is to evaluate the proposed development with regard for good urban design principles and ensure that the planned development appropriately addresses the City of Kitchener’s urban design policy framework and guidelines. This Urban Design Brief is being submitted in support of a Zoning Bylaw Amendment (“ZBA”) and Site Plan Approval (“SPA”) for the proposed development. This brief has been prepared in accordance with the urban design comments from “Record of Pre-Submission Consultation” meeting held on May 10, 2022.

The Kitchener Official Plan identifies Urban Design Brief as comprehensive documents “which may include urban design vision, principles, objectives, guidelines and strategies” and that “may be required of an owner/applicant in support of a development application”. Based on the matters for consideration and evaluation identified in the pre-submission consultation record, this Urban Design Brief contains:

- A description of the existing physical conditions on the Subject Property (Section 2);
- A description and characterization of the surrounding area and neighbourhood context (Section 3);
- A description of the design components of the proposed development (Section 4);
- An assessment of the proposed design concept in respect to relevant design policies and guidelines (Section 5); and
- A summary of the report findings (Section 6).

1.3 Supporting Studies and Materials

This Urban Design Report has considered the following plans and reports supporting the proposed application as well as relevant City policy and guidelines documents:

- Site Plan prepared by GSP Group; and
- Elevations prepared by Orchard Design Studio Inc.

2. EXISTING SITE CONDITIONS AND CONTEXT

2.1 Location and Description

The Subject Property is located on the north side of Bloomingdale Street in Bridgeport East neighbourhood of Kitchener. The Subject Property is 0.64 hectares (2.47 acres) in size and generally rectangular in shape, with a frontage of 67 metres on Bloomingdale Street.

2.2 Existing Site Conditions and Topography

There is an existing industrial building on the property, along with ancillary structures and a detached dwelling. All buildings on the property are proposed to be demolished during the construction process. The topography of the Subject Property is predominantly flat, and it is bordered by mature deciduous trees situated along the top of the bank on the north side of the property.



Site Area

2.3 Surrounding Neighbourhood Context

The Subject Property is located within the Bridgeport East neighbourhood of Kitchener, generally bound by the Grand River on the north, west and south sides, and the City of Kitchener municipal boundary on the east side. The urban fabric of this neighbourhood is irregular; a common characteristic of older low density neighbourhoods. The size, age, and form of the residential dwellings vary and include single detached dwellings and semi-detached dwellings of post-war architectural style. The neighbourhood includes select multiple-unit development forms including townhouses and low-rise apartment buildings such as 44 Nelson Avenue; 99, 103 Schweitzer Street; 126, 130, 134 Bloomingdale Road North.

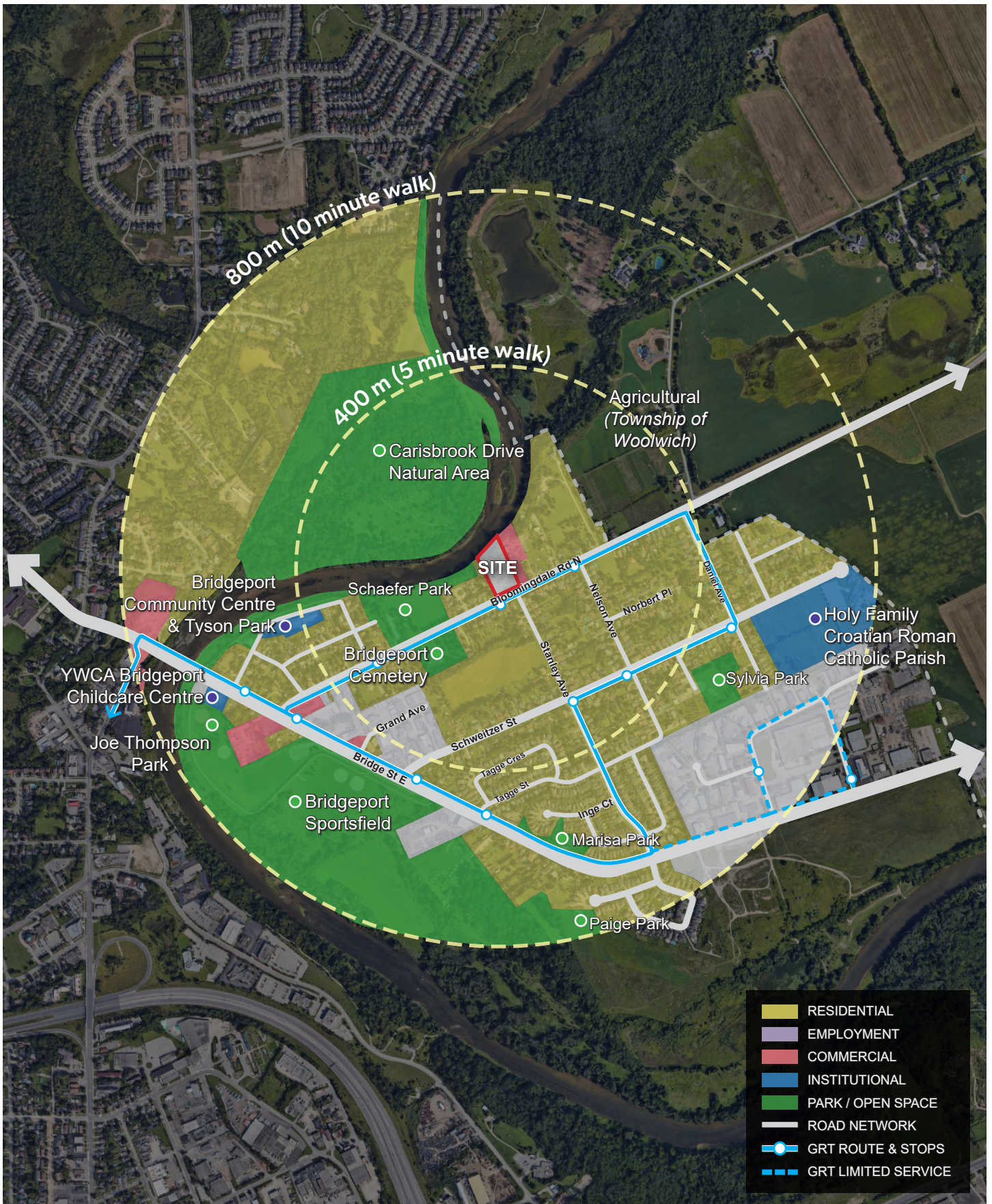
The Bridge Street East corridor at the south-west edge of the neighbourhood includes light industrial uses and park/open spaces, with public trail access along the Grand River. An expanse of the open

space area adjacent to the Grand River on the west and south side of the neighbourhood will remain essentially in a natural state due to its flood prone location within the floodplain.

There are multiple community facilities distributed throughout the neighbourhood including: 6 neighbourhood parks, Bridgeport Sportsfield, a community centre ("Bridgeport Community Centre") YWCA Bridgeport Childcare Centre, and a church ("Holy Family Croatian Roman Catholic Parish"), offering various trails and recreational opportunities. Bloomingdale Road is identified in the Cycling and Trails Master Plan as being part of the Bicycle Network and has on-street cycling infrastructure. GRT transit for route 5 (peak 30-minute headways) runs through the neighbourhood, with the closest bus stops to the Subject Property at the Bloomingdale/Stanley and Stanley/Schweitzer intersection (within a five-minute walk).



6-plex buildings at 126, 130, 134 Bloomingdale Road North, located within 100 metres of the Site



Neighbourhood Context surrounding Site

2.4 Abutting Property Context

The surrounding area is mainly characterized by residential, light industrial uses, open space land uses and environmental features. More specifically, uses that surround the Subject Property include the following:

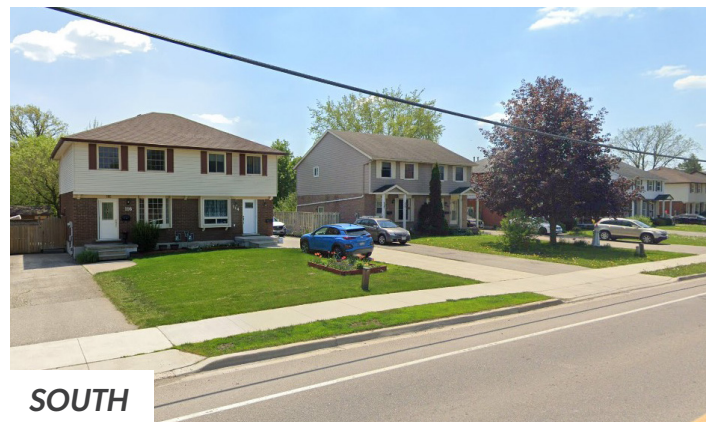
North: The Subject Property is located immediately adjacent to the Grand River to the north. A portion of the Subject Property along the northern limit is within the Grand River Conservation Authority (GRCA) Regulation Limit and contains portions of Slope Erosion, Slope Valley and Floodplain Natural Hazard features. The Proposed Development is setback 12 metres from the top of bank as well as the Significant Valleyland features along the northern property limit.

East: To the immediate east of the Subject Property is an excavation dispatch facility (Richard Schaefer & Son Haulage Limited). The facility provides excavation and construction services, and contains a dump truck, a tow truck, excavating equipment, storage and maintenance sheds, and small outdoor stockpiles of materials. This Subject Property is considered as a Class I or Class II facility and the proposed development is 9 metres away from the facility. Mitigating measures such as a noise barrier wall of 3 metres in height are required along the east property line and no windows to be proposed on the east façade of the northernmost townhouse block.

South: The Subject Property is situated at the termination point of Stanley Avenue. On the south side of Bloomingdale Street, there are residential dwellings that do not have car garages. Instead, the individual driveways of these properties are utilized for parking cars. The residential lots located at the corner of Stanley Avenue and Bloomingdale Road feature detached dwellings with frontages along Stanley Avenue.

West: Abutting the Subject Property at the west is a residential property setback approximately 27 metres from the public sidewalk to the building line. The residential property consists of a 2-storey detached dwelling with an outdoor storage structure at the rear (107 Bloomingdale Road). Currently, there are no existing trees or fencing

along the shared property line to separate the Subject Property from this property.



3. DESIGN POLICY AND GUIDELINE REFERENCES

3.1 City of Kitchener Official Plan

Low Rise Residential Policies

The Site is part of the Community Areas structure element and is designated Low Rise Residential in the City of Kitchener Official Plan (“Official Plan”). The Low-Rise Residential designation is intended to accommodate a range of low density housing types including single detached, semi-detached and street townhouses; specifically, the designation encourages and supports “the mixing and integration of innovation and different forms of housing to achieve and maintain allow-rise built form” (Policy 15.D.3.9). A maximum building height of 4 storeys or 14 metres, at the highest grade elevation, whichever is the lesser, may be permitted on lands having primary frontage on to a Regional Road or City Arterial Street (Policy 15.D.3.13).

General residential policies direct “the successful integration of different housing types” through intensification within the Low Rise Residential designation (Policy 15.D.3.3) in keeping with the following considerations:

- *Compatibility of building form with respect to massing, scale, design;*
- *The relationship of housing to adjacent buildings, streets and exterior areas;*
- *Adequate and appropriate parking areas are provided on site;*
- *Adequate and appropriate amenity areas and landscaped areas are provided on site.*

3.2 General Urban Design Policies

Section 11 of the Official Plan contains general urban design policies that are used to evaluate movement patterns, the relationship between built form and open spaces, integration of natural and cultural resources and development impacts. They include:

General urban design policies that speak to the matters such as streetscapes, CPTED principles, fire

prevention, barrier-free accessibility, and shade.

Neighbourhood Design policies that provide direction for the design of new neighbourhoods, including design objectives of walkability, variety, place making, conservation, connectivity, transit-supportive and safety. The City’s primary focus is to create diverse, attractive, walkable neighbourhoods that contribute to complete and healthy communities.

Building Design, Massing and Scale design policies speak to human-scale proportions to support a comfortable and attractive public realm, including attractive building forms, façades, and roof designs; complementary design of new buildings; and architectural innovation and expression.

Section 17.E.10.5 identifies that urban design briefs/reports together with other design-related materials are meant to be used to:

- a) demonstrate that a Proposed Development or redevelopment is compatible;
- b) address the relationship to and the privacy of adjacent residential development; and,
- c) ensure compatibility with the existing built form and the physical character of the established area and/or neighbourhood.

3.3 Urban Design Manual

PART A -Design Guidelines

Part A contains design guidelines on various land uses, built types, geographic areas, and urban structure elements. The following topics of design guidelines are relevant to the Site and the proposed building.

a) City-Wide

The City-Wide design guidelines apply to Kitchener as a whole. The main objective of these guidelines is to ensure Kitchener is designed as an inclusive, safe, accessible, comfortable, and appealing place

to live, work and play. Guidelines are divided into Community Design and Site Design. The Community Design guidelines are primarily used by the City in designing the form and structure of communities through the application of design best practices in a range of topics. The Site Design guidelines address built form, open space and site functionality.

b) New Neighbourhoods

The New Neighbourhood design guidelines apply to undeveloped lands including in the Community Areas of the City's Urban structure of the Official Plan. They are generally to direct development of new communities or neighbourhoods by Plan of Subdivision or Condominium.

The New Neighbourhoods guidelines is composed of nine objectives: walkability, variety, placemaking, conservation, connectivity, transit supportive, safety, viability and liveability.

c) Low-Rise Multiple Residential Buildings

The Low-Rise Multiple Residential Buildings design guidelines apply to multiple residential and townhouse buildings, generally 3 storeys or less. The guidelines note that such forms are a "valuable alternative to taller forms when seeking to achieve greater densities in established or new low-rise neighbourhoods". The Low-Rise Multiple Residential Buildings guidelines address compatibility, building components and site function.

PART C – Design Standards

Part C contains design standards with specifications on technical details. Several standards are applicable to the Proposed Development, including those for access to roads, surface parking, outdoor lighting, barrier-free accessibility, pedestrian and transit-supportive development, emergency services, multiple residential, landscaping and natural features, and landscape design.

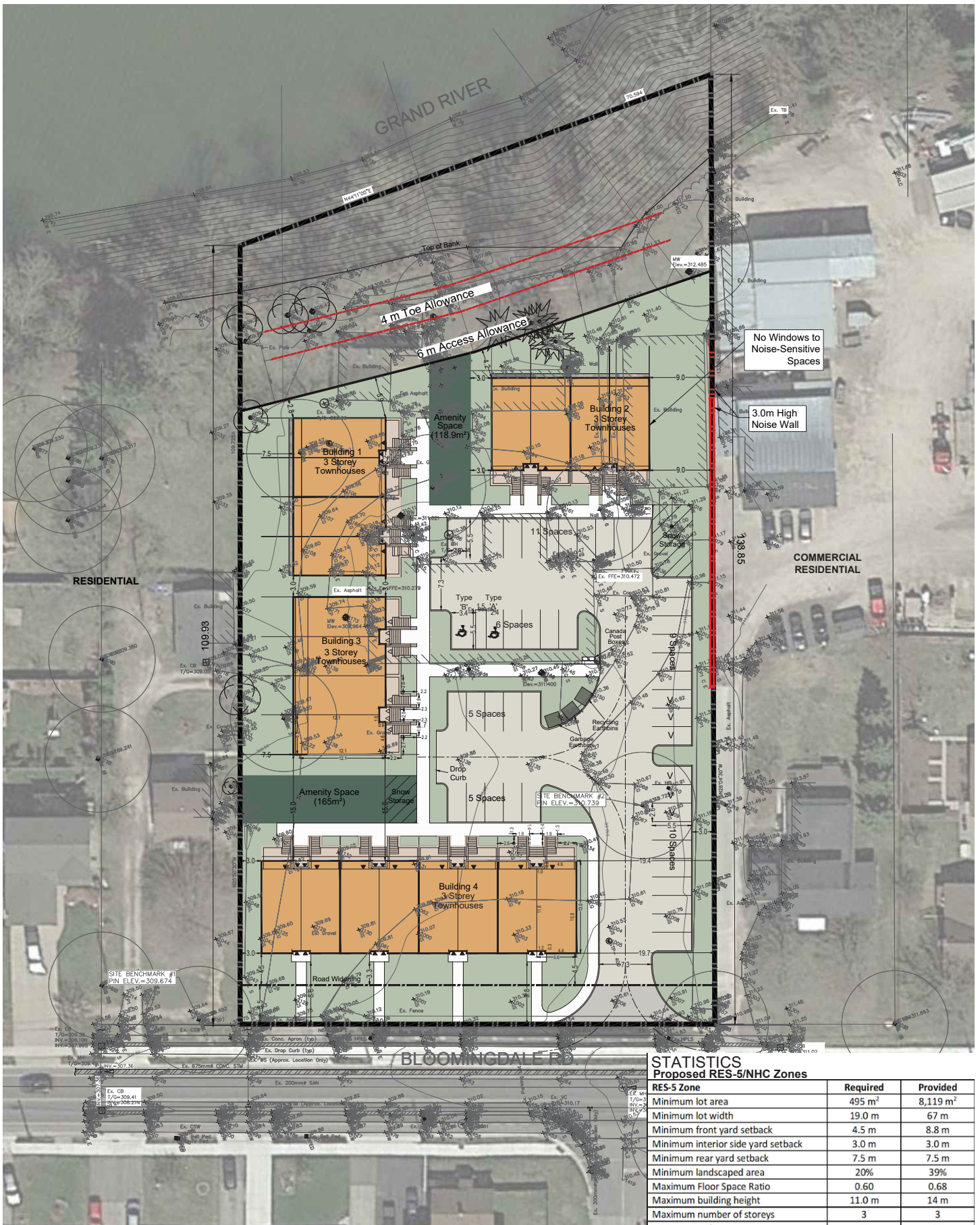
4. PROPOSED DEVELOPMENT

4.1 Design Vision and Objectives

The Subject Property envisions to be developed into a high-quality 3-storey townhouse buildings consisting of 40 units with outdoor amenity area of 284 square metres and a total of 46 surface parking spaces including two barrier-free parking spaces. The design goal for the proposed development is to make efficient use of the Subject Property by intensifying with a compact form of development that fits contextually with the character of Bloomingdale Road and the broader neighbourhood.

To realize this goal, the objectives for the proposed development are to:

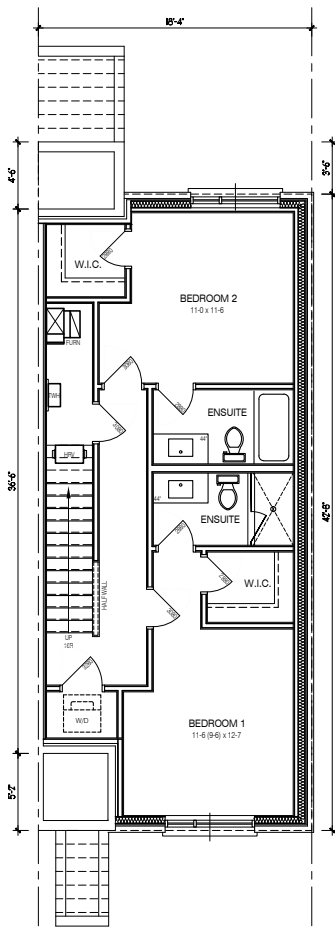
- Facilitate a compact urban form that reinforces human scale proportions.
- Orient the building to address Bloomingdale Road streetscape with an architectural rhythm that provides interest along the pedestrian realm.
- Achieve a high-quality of architectural design and construction that is harmonious with the distinct identity and style of the adjacent buildings.
- Ensure safety and security by utilization of CPTED principles.
- Minimize the impact of utilities and services on the pedestrian realm within the Subject Property.



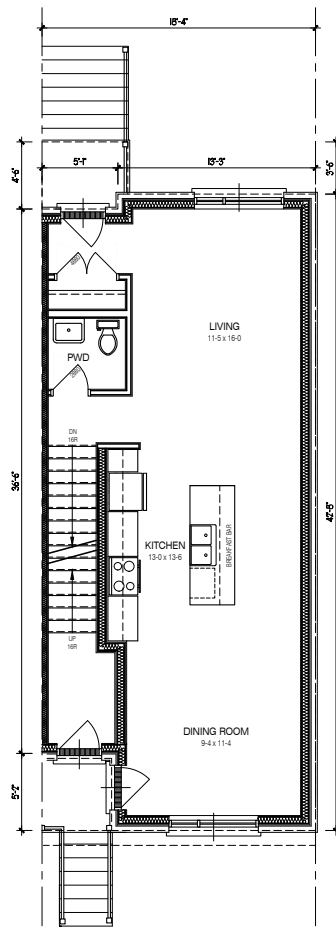
STATISTICS
Proposed RES-5/NHC Zones

RES-5 Zone	Required	Provided
Minimum lot area	495 m ²	8,119 m ²
Minimum lot width	19.0 m	67 m
Minimum front yard setback	4.5 m	8.8 m
Minimum interior side yard setback	3.0 m	3.0 m
Minimum rear yard setback	7.5 m	7.5 m
Minimum landscaped area	20%	39%
Maximum Floor Space Ratio	0.60	0.68
Maximum building height	11.0 m	14 m
Maximum number of storeys	3	3
Minimum parking spaces	46	46
Minimum bicycle parking stalls (Class A)	20	
Minimum bicycle parking stalls (Class B)	6	

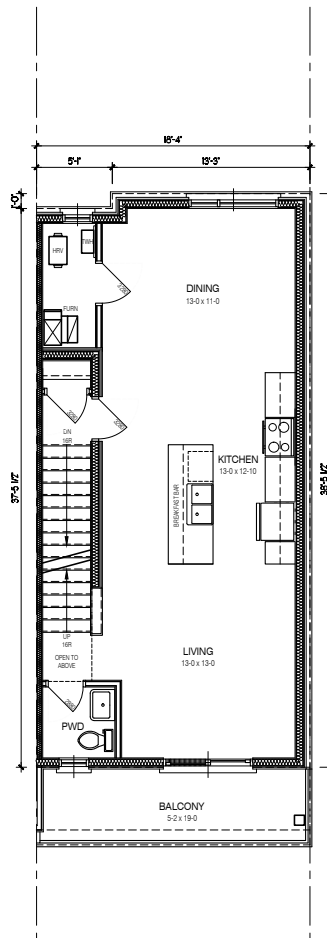
Site Plan prepared by GSP Group



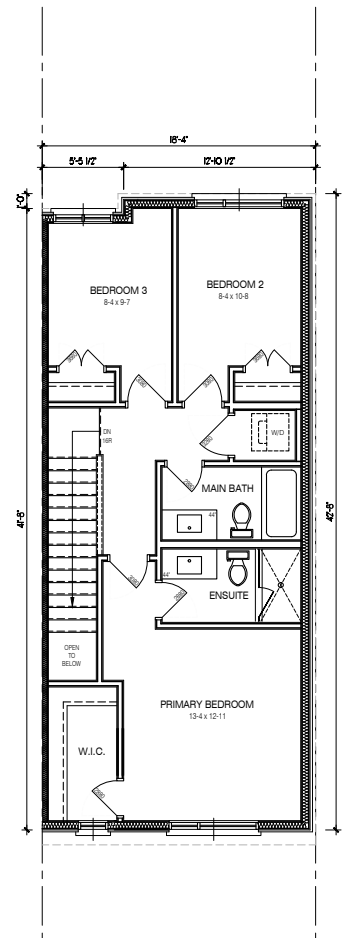
Level 1 Floor Plan - Unit 1
Area: 753 sq.ft



Level 2 Floor Plan - Unit 1
Area: 689 sq.ft



Level 3 Floor Plan - Unit 2
Area: 766 sq.ft



Level 4 Floor Plan - Unit 2
Area: 754 sq.ft

Floor Plans prepared by Orchard Design Studio Inc.

4.2 Building Positioning

Building 4, townhouse block, comprises 8 units in a row. It is strategically positioned to create a continuous frontage along Bloomingdale Road, with a setback of 4.5 meters from the post-road widening property line (10 meters from the current property line). These setbacks are designed to maintain a consistent “street wall” appearance and contribute to the vibrancy of the street, while also allowing ample space for landscaping and concrete walkways. Building 4 is set back 19.7 meters from the east property line to accommodate a driveway access into the Subject Property.

Buildings 1 and 3, each consisting of 4 units in a row, are set back 7.5 meters from the west side of the property line. The main entrances to these buildings are accessed from the internal walkway. Building 2, which comprises 4 units in a row, is positioned on the north side of the Subject Property with a setback of 4.2 meters from the edge of the block to the rear lot line.

The proposal has been oriented to successfully integrating with its surroundings, and high quality architectural design is provided for each building elevation so that the proposed development contributes to the physical character of the community.

4.3 Building Scale and Massing

All four buildings are 3-storeys, measuring 12.98 metres in height to top of ridge for Building 4 and 12.45 metres for Buildings 1-3. The proposed buildings occupy 1,422 square metres of building footprint, which is approximately 22% of the lot area. The stacked townhouse block consists of a lower unit with ground level and basement level and the upper unit with two uppermost levels. Units are approximately 5.5 metres wide each. Building 4 has entrances facing Bloomingdale Road and Buildings 1-3 have entrances facing the internal driveway. Building entrances are located above-grade, accessible via steps. They are recessed and weather protected by extended upper storeys.

4.4 Parking

Parking for the proposed development consists of 46 surface parking spaces, including two barrier-free spaces. The surface parking area is located internal to the Subject Property to buffer its visual impact from the public streetscape. There is sufficient space to accommodate surface-mounted bicycle racks for short-term bicycle parking near the buildings. Secure bicycle parking for residents would be accommodated within individual units.

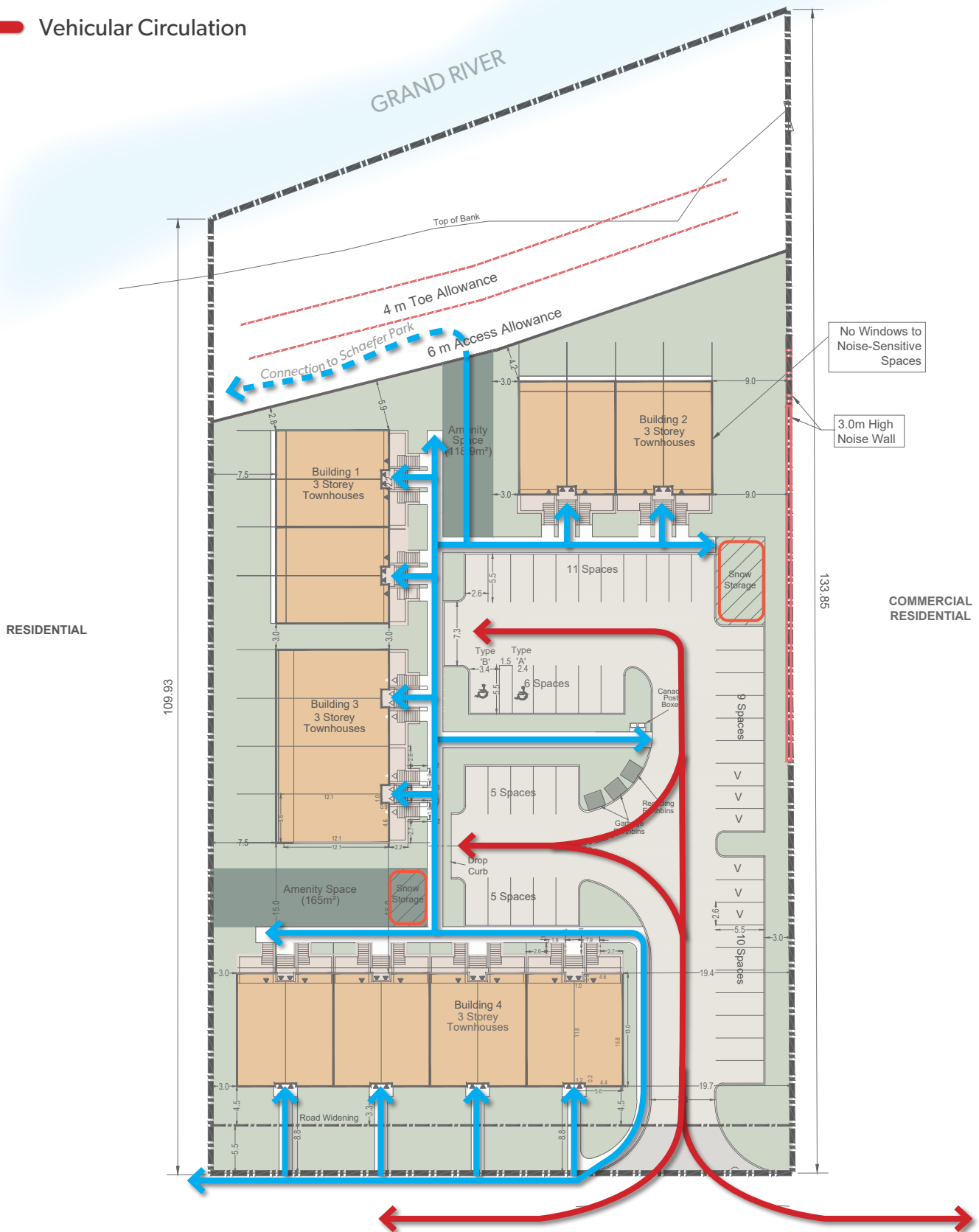
4.5 Access and Circulation

Pedestrian circulation through the Subject Property is integrated into the design of the parking area, sufficiently buffered, and protected from vehicle areas. Paved walkways are planned for the individual units that face Bloomingdale Road. Additionally, an internal 1.8-metre-wide sidewalk will extend from the public sidewalk on Bloomingdale Road, providing pedestrian access throughout the Subject Property.

4.6 Service Areas

Two deep well garbage enclosures and a recycling bin are located within the parking area, easily accessed and traversed by the driveway. There is sufficient space to accommodate snow storage on-site and the designated locations for these areas are clearly marked on both the Site Plan and the Circulation Diagram.

- █ Pedestrian Circulation
- █ Vehicular Circulation



Circulation Diagram

4.7 Building Articulation

The preliminary building elevations draw on material palettes and forms from the established character in the surrounding neighbourhood in terms of form, materiality, and colouration. The below descriptions are the main architectural effects for the Proposed Development:

- Brick in royal grey colour is used as the principal material for the lower units, tying to the prevailing neighbourhood palette, whereas the upper units are treated with James Hardie Lap Siding in boothbay blue colour.
- No blank facades: all elevations are detailed with glazing, articulation, and a mix of materials.
- Windows and doors are composed with a regular rhythm along the building lengths and ends to the extent possible.
- Lintels and trim surround windows and doors for accent and interest. Contrasting trim colour but compatible with the primary building material colour.
- Distinguished horizontal lines throughout the building elevations provide a vertical distinction between the storeys.
- Recessed unit entrance doors for weather protection, balconies for upper storeys with an integrated railing system for natural surveillance.

4.8 Streetscape and Landscape Design

The proposed development will include landscape buffer areas around the street facing property line and along perimeter of the Subject Property; landscaping on the Subject Property is generally limited to such areas given the form and footprint of the buildings. Decorative pavements and planter beds will be proposed along the base of the buildings to soften the building edges. Tree planting, shrub/perennial plantings, and landscaped entry features will be included to animate the streets and screen undesirable views such as utility equipment. Water efficient and drought resistant landscaping will be considered, including using native planting materials.

4.9 Outdoor Amenity Area

There are two linear common amenity areas proposed between Buildings 3/4 of 165 square metres and Buildings 1/2 of 119 square metres. The amenity areas are located internal to the development blocks such that they are sheltered from noise and traffic of nearby public streets. The outdoor amenity space will include a combination of hard surface and landscaped areas. They may include paved pedestrian walkways, trellis/shade structure, outdoor cooking area, seating area, site furniture, and ornamental planting.

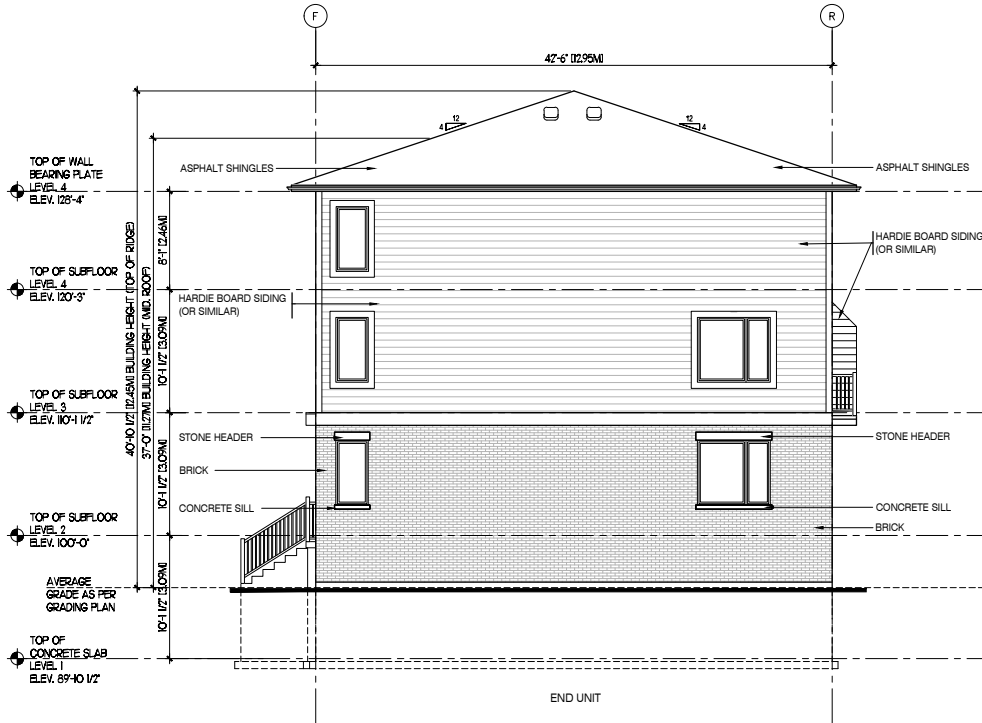


Front Elevation

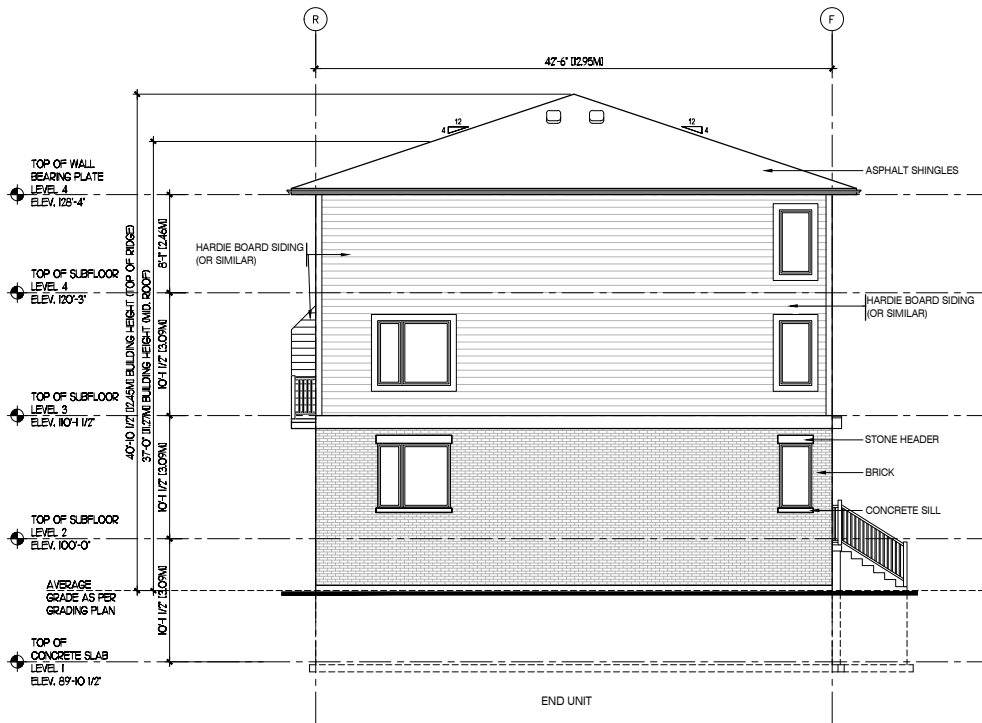


Rear Elevation

Buildings 1-3 Elevation prepared by Orchard Design Studio Inc.



Right Side Elevation



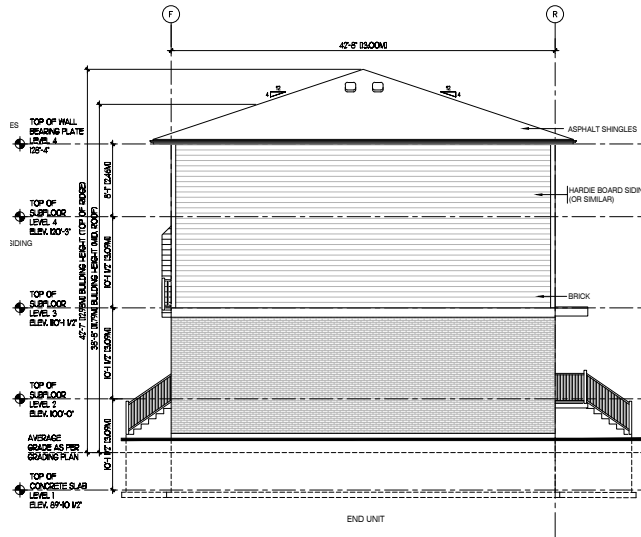
Left Side Elevation



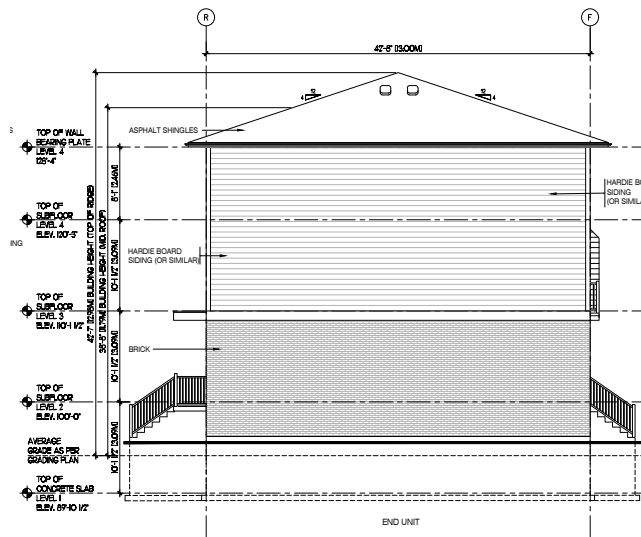
Front Elevation
(facing parking lot)



Rear Elevation
(facing Bloomingdale Road)



**Right Side Elevation
(facing parking lot)**



**Left Side Elevation
(facing parking lot)**

5. RESPONSE TO POLICY AND GUIDELINE FRAMEWORK

5.1 Response to Official Plan Policy

The proposed development is sensitive and compatible with the surrounding context of immediate properties and the broader Bridgeport East neighbourhood. The proposed design responds to the design direction of the Low Rise Residential designation by supporting a mix and integration of innovative and different housing forms achieving a low-rise pattern. The integration satisfies intensification considerations for the Low Rise Residential designation through the following:

- a) Compatible building form with inclusion of stacked townhouse dwellings;
- b) A buffered relationship to the adjacent residential dwellings (east, south and west) through a combination of building siting and perimeter landscape treatments;
- c) Adequate parking provided to the development through surface parking spaces;
- d) Common amenity areas are proposed on-site and private amenity areas are provided for upper units via individual balconies.

The proposed low rise buildings are designed in keeping with the design policy direction of Official Plan Section 11, as further described in detail as part of the Urban Design Manual analysis in the following section.

This Urban Design Brief, per Official Plan Section 17.E.10.5, demonstrates that the Proposed Development is a compatible addition to the Bridgeport East community.

5.2 Response to Urban Design Guidelines

Inclusive Design

CITY-WIDE

LOW RISE MULTIPLE RESIDENTIAL

The proposed development supports the Crime Prevention through Environmental Design (CPTED) design principles including opportunities for informal surveillance by providing “eyes on the street” through townhouses that face and front the street, visible entrances with porches/stoops and large windows overlooking the street. With the interior layout of the buildings, there will also be eyes on the parking area and the intent is to make the common outdoor amenity area an inviting place to gather.

- Through the Site Plan Approval process, the landscape design will consider visibility, sight lines and the delineation between the public and private realms.
- Pedestrian-scale lighting will be defined at the detailed design stage, focusing on internal site walkways and surface parking areas.
- Internal sidewalk connections will be designed for barrier-free accessibility at the time of detailed design.

Design for Sustainability

CITY-WIDE

LOW RISE MULTIPLE RESIDENTIAL

- The proposed development represents compact intensification of an underdeveloped site within an established area served by existing transit and community uses.
- Subject Property is transit supportive and walkable and provides cycling infrastructure. Subject Property is a short walk (5 minute walk) to local bus transit stops and Bloomingdale Road has on-street cycling infrastructure
- The proposed height and mass of buildings will not create negative microclimates within the site or on adjacent properties. All units are provided with operable windows for natural ventilation/cooling when possible.
- The proposed development has proposed deep well waste and recycling collection areas to

encourage the collecting and recycling of waste produced by residents.

- Details regarding the landscaping plan will be provided during Site Plan Approval but measures which will utilize drought and salt resistant, low maintenance plants as well as the appropriateness for pollinating supportive plantings will be considered.

Design for Outdoor Comfort

CITY-WIDE

LOW RISE MULTIPLE RESIDENTIAL

- The proposed development design addresses pedestrian weather protection through covered entranceway and landing spaces.
- The pedestrian pathway will be finished with a non-slip surface treatment and will be maintained to accommodate pedestrians during all weather conditions.
- Lighting and landscape plans will address pedestrian comfort guidelines at the time of detailed design.

Street Design

CITY-WIDE

LOW RISE MULTIPLE RESIDENTIAL

- The proposed development will include soft landscaping elements that will complement the streetscape.
- Where driveways for the proposed development intersect public sidewalks, the sidewalks will be demarcated in a different surface treatment and will maintain barrier-free accessible standards.

Compatibility

CITY-WIDE

LOW RISE MULTIPLE RESIDENTIAL

- The proposed buildings oriented parallel to the western property line abuts the rear yard of the existing detached dwelling to the west. The proposed buildings are setback 7.5 meters from the property line, ensuring sufficient setbacks to mitigate impacts and provide transition between the buildings and the neighbouring properties.
- The proposed buildings on the Subject Property are strategically positioned along the northern, western, and southern property lines, with the

southern side facing the street, framing the internal surface parking area.

- The proposed buildings, with its 3-storey height, are designed to be compatible with its surroundings and are appropriately situated in relation to adjacent properties. The elevations of the buildings are characterized by a highly articulated design, incorporating a regular rhythm of vertical and horizontal elements such as fenestration and variations in materials. The inclusion of balconies and extensive glazing enhances natural surveillance of the public realm and parking area.
- Human-scaled architecture achieved with brick base material, vertical divisions, generous fenestration patterns throughout, and the recessed building entrances.

Built Form

CITY-WIDE

- The proposed buildings are oriented and positioned to respect the local context, including the interface with the low-rise residential neighbourhood to the south and west.
- Buildings 1-3 which are 3 storeys in heights are positioned on the north and west side of the property lines to create a transition zone between the proposed development and the existing detached dwelling to the west. This positioning considers the impact on the neighbouring property while concentrating the height and mass towards the interior of the property, potentially minimizing unwanted impacts on the surrounding areas. Building 4 is three storeys in height and is positioned along the street facing property line to maintain a consistent street continuity along Bloomingdale Road.
- Architectural detailing, including balconies, projections and materials are used on the north, east and west elevations which help reduce the apparent scale of the building.
- Balconies, windows and variation of building materials and articulated architectural projections are incorporated to create visual interest and reinforce human scale.

- All elevations, including side elevations, have window openings and balconies; no blank facades.

Shared Spaces

CITY-WIDE

LOW RISE MULTIPLE RESIDENTIAL

- The lower units will have access to individual rear yards and the upper units will have individual balconies on the front wall facing the surface parking area and the street. In addition, to that, there are two common amenity areas of 165 square metres and 119 square metres, proposed between Buildings 3/4 and Buildings 1/2, respectively.
- The common amenity areas will be safe and accessible and does not have to cross drive aisles or parking areas to access amenity areas.
- The common amenity areas will include a pedestrian connection to the adjacent Schaefer Park.
- Streetscape treatment along Bloomingdale Road will provide opportunity for hard and soft landscape treatments.

Site Function

CITY-WIDE

LOW RISE MULTIPLE RESIDENTIAL

- Parking area are internalized on the Subject Property behind the buildings screened from the street and from the residential properties to the west. Single curb cut is proposed along Bloomingdale Road, minimizing the number of access points, and promoting a more efficient use of space.
- The proposed development includes a single site access and a driveway width of 7.3 metres. Careful consideration has been given to balancing the need for vehicular access with the desire to incorporate landscaping and stormwater management strategies.
- The parking area are strategically concealed from the street and neighbouring residential properties to the west. The portion of the parking area that is exposed to the abutting industrial site to the east will be screened using landscape treatment.

- There is sufficient space to accommodate surface-mounted bicycle racks for short-term bicycle parking near the buildings. Secure bicycle parking for residents will be accommodated within individual units.
- Utility infrastructure will be located at time of detailed design.
- Separate 1.8-metre-wide walkways are proposed to line the building faces along the parking area edge connecting to the public sidewalk on Bloomingdale Road, ensuring a clear separation between the pedestrian walkways and parking area.
- Fully enclosed deep well garbage enclosures are located in accessible location for building residents and collection vehicles, connected to site walkways without obstructing vehicular routes.
- Entrances and landing spaces in front of entrance will be designed to universal accessibility standards.
- and balconies provided for each upper unit.
- Siting of the buildings on the Site provide for screening of site functions such as loading, parking and garbage areas from the public realm.
- Paired unit entrances are clearly visible from the surface parking area and connect directly to walkways (1.8 metres wide) leading to the public sidewalk on Bloomingdale Road.

Building Components

LOW RISE MULTIPLE RESIDENTIAL

- The proposed buildings incorporate pitched roofs that are consistent with the surrounding architectural styles and visual language of the neighbourhood.
- Living rooms for lower unit and upper unit occupy most of the unit width on ground floor and second floor, respectively, providing significant views over shared surface parking area and site walkways.
- Materials are used to accentuate the depth of the building elevations coinciding with projecting elements and to distinguish between the building base and the upper storeys to reduce the perception of mass.
- Utility and HVAC locations are not known at this time.
- Elevations use high quality brick and siding materials through the building cladding, demonstrating variation in colours, materiality and textures.
- On the front walls, units share recessed entrances

6. CONCLUSION

The proposed development at 117 Bloomingdale Road is a multiple residential project consisting of four blocks of stacked townhouses with 8 units horizontally along the street and 4 units horizontally for the blocks situated internal to the Subject Property. Generally, the blocks are 3 storeys in height with a total of 40 townhouse units and at-grade common amenity areas. A surface parking lot with 46 parking spaces is located centrally within the property, mostly bound by the buildings on the west and south sides. Internal site walkways lead from the public sidewalk on Bloomingdale Road to the internal face of each building block.

It is the opinion of this Urban Design Brief that the form and character of the proposed development represent good urban design. The placement, scale, massing, and character of the proposed buildings are compatible with the surrounding neighbourhood character. The proposed built form succeeds in enhancing the character of the Subject Property at a prominent location within Bridgeport East neighbourhood, while still being respectful of the surrounding context.

The design of the proposed buildings is at appropriate heights and scales and defines the public realm with good proportions. The proposed stacked townhouses will have a high standard of design and quality. There are no blank walls proposed, as all visible facades of the building contain projections and recessions as well as more than one type of material used. The functionality of the Subject Property has been thoughtfully considered and integrated with the adjacent uses.

In conclusion, the proposal fits with the urban design framework of the Official Plan and design directives from the Urban Design Manual by providing a low-density form that enhances the public realm and is compatible with the surrounding area. The proposed building includes at-grade residential units with direct access from the public sidewalk and will help to activate the streetscape.

The development supports the objectives of the City's Official Plan to achieve a high standard of urban design, architecture and place-making that contributes positively to quality of life, environmental viability, and economic vitality. The proposal supports the City's overarching design directives by proposing a residential development that will reflect a high standard of design excellence; is visually distinctive and has an identifiable sense of place; is human-scaled, safe, secure and walkable; respects and enhances adjacent natural areas; is mutually supportive with the adjacent multi-residential development and existing single family homes, and; minimizes and mitigates adverse impacts.

In conclusion, the proposed design has considered and achieves the intent of the urban design policies outlined in the Official Plan and design directives from the Urban Design Manual and therefore should move forward through the Zoning Amendment and detailed Site Plan Process.

